

PARKLETS

Temporary Sidewalk Extension (TSE) Policy

What is a Parklet?

Parklets are public seating platforms that convert curbside parking spaces into vibrant community spaces. Also known as street seats or curbside seating, parklets are the product of a partnership between the city and local businesses, residents, or neighborhood associations.

Most parklets have a distinctive design that incorporates seating, greenery, and/or bike racks and accommodate unmet demand for public space on thriving neighborhood retail streets or commercial areas. A Parklet may be thought of as semi-permanent, but must be designed for quick and easy removal in case of an emergency, or other reasons such as utility work, without damaging the curb or street.

How do I obtain permission for a Parklet?

Parklets are typically allowed where narrow or congested sidewalks prevent the installation of traditional sidewalk cafes, or where local property owners or residents see a need to expand the seating capacity and public space on a given street. More details are available on the following pages of this guide. **To obtain permission for a parklet, property owners enter into an agreement with the City of Savannah, through the eProval application process, procuring curbside seating in place of one or more parking spaces**

Parklets are administered through a City partnership with businesses and/or surrounding residents. Partners maintain and program the parklet, keeping it free of trash and debris. Where no local partners are present, a parklet may be installed and managed by the City as a traditional park or public space

Costs vary based on the design and size of the parklet. Design and installation costs are generally assumed by the maintenance partner.

While parklets are foremost intended as assets for the community, their presence has also been shown to increase foot traffic, and in some cases revenues, for adjacent businesses.



Illustration only. Not Drawn to Scale

PARKLETS_{continued}

Approved Parklet Locations

Except in exceptional circumstances, Parklets shall only be permitted in locations that satisfy all of the following criteria.

- Within on-street parallel or angled public parking spaces;
- In areas with existing pedestrian activity due to the presence of retail, mixed-use, commercial or high-density residential development;
- On streets with a posted speed limit of 25mph or less (30mph on a case-by-case basis); and,
- In spaces where parking is permitted during all hours of the day/night.

Prohibited

- Any State Route;
- Broughton Street;
- Within 10' of a City stormwater catch basin;
- Within 30 feet of stop signs, yield signs or traffic signals;
- Within 20 feet of crosswalks;
- Within 15 feet of driveways or bus stops;
- In parking spaces with a running slope greater than or equal to 5%;
- Above utility holes or other utility access;
- Within 15 feet of fire hydrants, stand pipes, or Fire Department connections;
- Where resurfacing or other street improvements are scheduled to occur within 1 year;
- In loading zones, freight zones, tour stands, or accessible parking spaces; and,

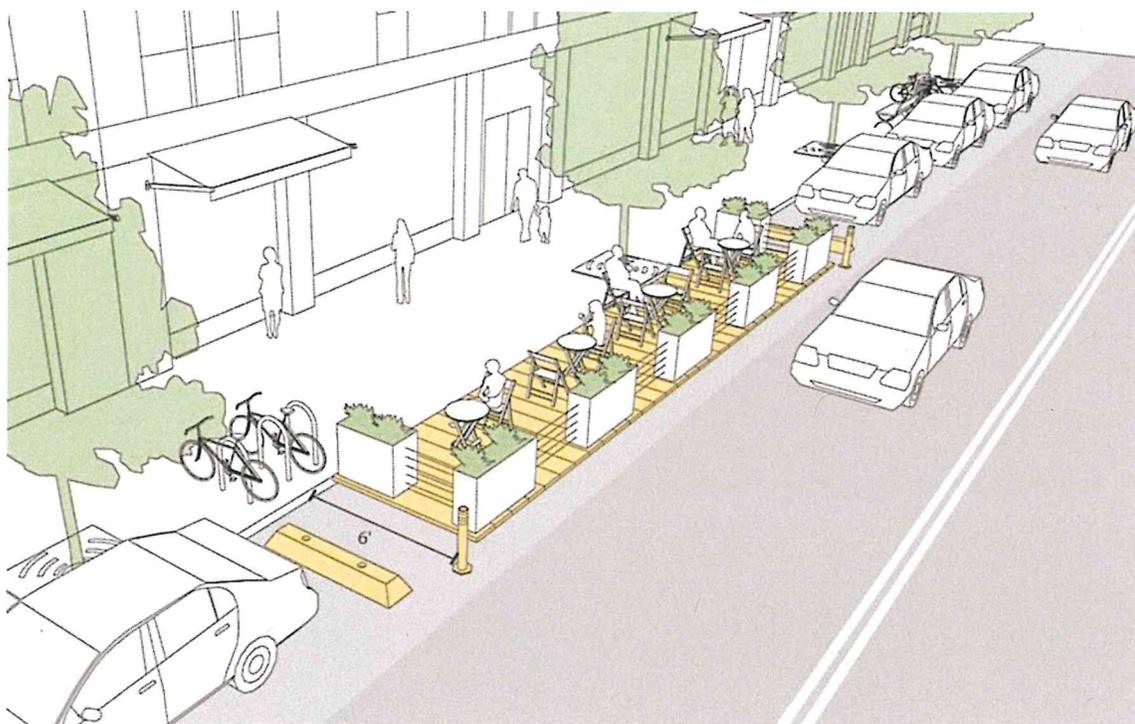


PARKLETS_{continued}

Design Guide

Dimensional Requirements

- To ensure visibility to moving traffic and parking cars, parklets must be buffered using a wheel stop at 4 feet from each end of the parklet.
- Parklets shall have a minimum width of 6 feet, including all associated buffers and materials, but shall not extend beyond the width of the parking stall.
- Parklets may convert up to two (2) consecutive parallel parking spaces or up to four angled parking spaces, and may vary according to the site, context, and desired character of the installation, as permitted by the City.
- Additional shy distance from travel lanes may be required when the adjacent street has a speed of 25mph or greater.
 - 25mph roadway—1' minimum buffer from adjacent travel lanes
 - 30mph roadway—2' minimum buffer from adjacent travel lanes
- Any overhead elements projecting over the sidewalk must provide a minimum of 108 inches of vertical clearance and may not interfere with the visibility of any traffic or wayfinding signage.
- Where a parklet stretches the length of an entire curb, accessibility and sightlines shall be taken into account.

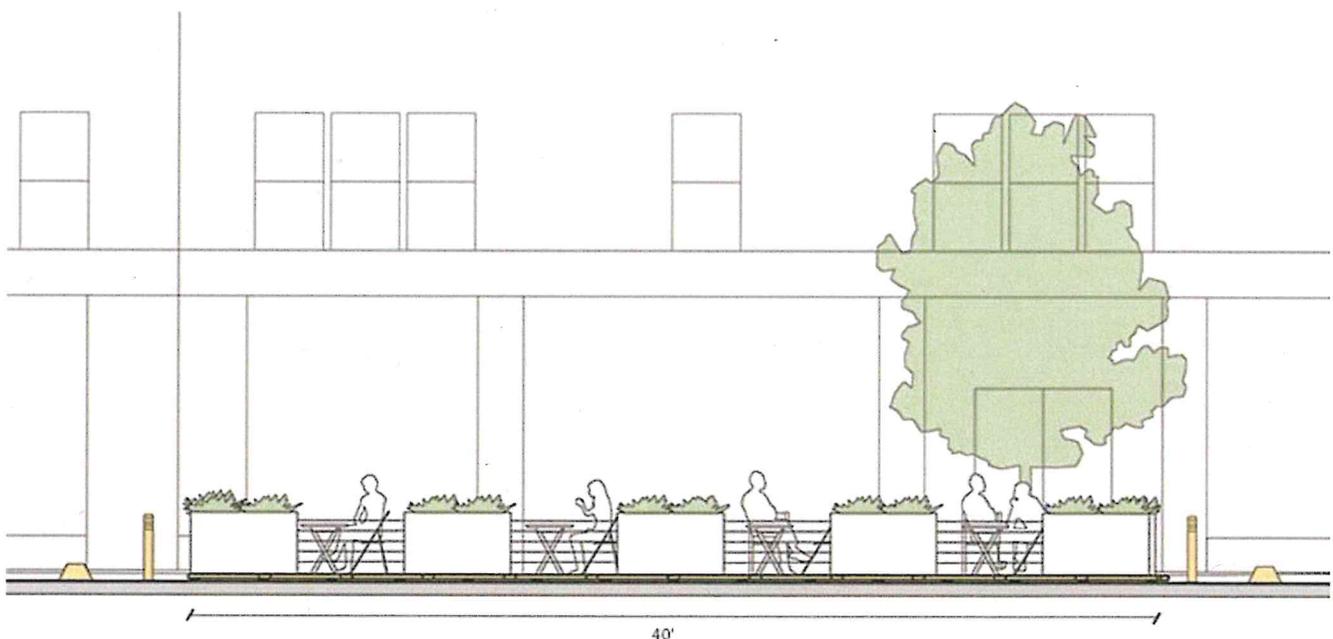


PARKLETS_{continued}

Design Guide

Design Requirements

- Parklets should be designed to be visually distinguishable from any adjacent private cafes or seating areas.
- Parklets shall have vertical elements that make them visible to traffic, such as flexible posts or bollards.
- Parklets shall maintain a visual connection from the street to the sidewalk.
- While buffers are required (see next section), Parklets must remain visually permeable to allow views into the space from both the sidewalk and roadway. Continuous opaque walls above 36 inches that block the view from the street into the Parklet are prohibited.
- Parklets shall be buffered from adjacent parking spaces and vehicular travel lanes. Such buffers may take the form of planters, railing, cabling, or other mechanisms deemed appropriate by the City.
- Parklets must have the ability to be completely removed within 48 hours of notice without damaging any curb, sidewalk, drainage structure, or underlying roadway. (If removal is required, all parklet items and materials must be stored and secured out of the City's right-of-way.)
- No portion of the Parklet may be glued, nailed, bolted, cemented, or otherwise affixed to the street or curb.
- Parklets must maintain adequate sight distance from any driveway or street intersection

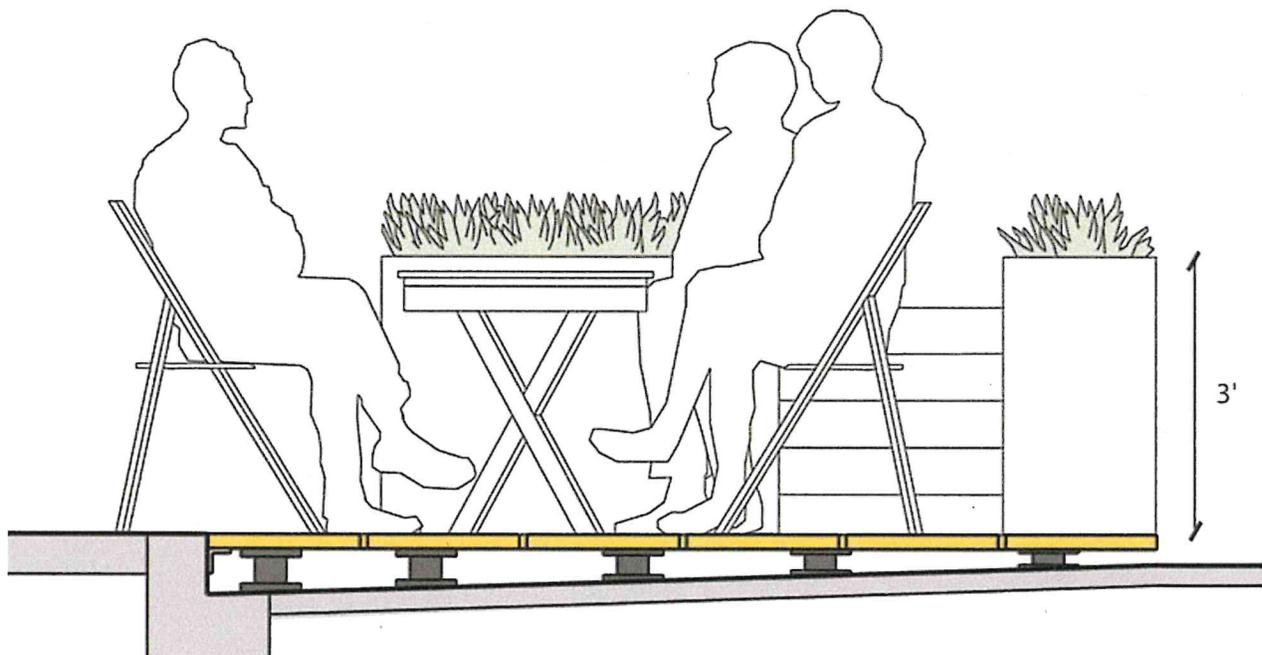
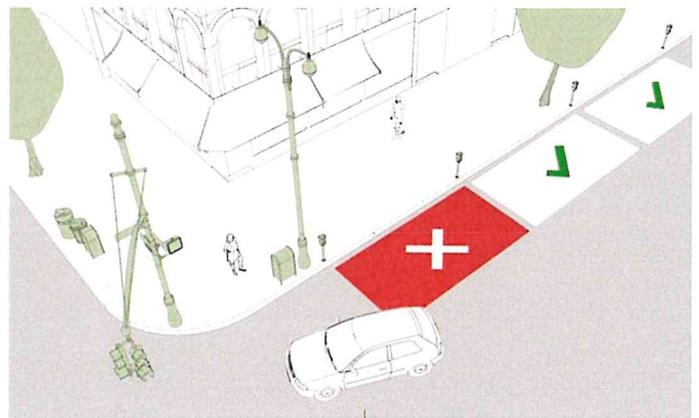


PARKLETS_{continued}

Design Guide

Construction Requirements

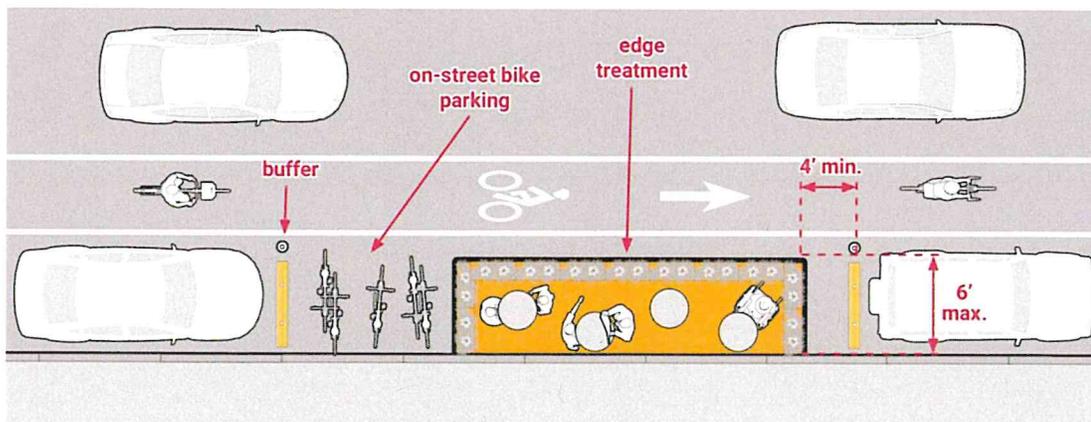
- Parklets must be constructed to be load bearing to a maximum of 750 lbs. per square foot.
- Parklets shall maintain a flush transition at the sidewalk and curb to permit ease of access and avoid tripping hazards.
- The design of a parklet shall not inhibit the adequate drainage of stormwater runoff. Channels between the base street and the platform must allow for the free flow of stormwater to facilitate drainage.
- Sheet flow drainage in streets and curb lines shall not be obstructed.
- Stormwater structures, to include catch basins and manholes, shall not be covered or obstructed.
- Each Parklet shall be designed to be accessible in accordance with the Americans with Disabilities Act (ADA).
- Parklet platforms must not exceed a 2% cross slope.
- Reflective elements shall be installed on Parklet corners for visibility.



PARKLETS_{continued}

Recommendations

- Where installation of a parklet is under consideration for a site near an intersection, volumes of turning traffic, sightlines, visibility, and daylighting should be taken into account.
- Parklets should be heavy enough to make theft impossible or unlikely. Site selection should consider the level of surveillance both during the day and at night.
- Incorporate seating into the parklet. Seating may be integrated into the design itself or made possible with moving tables and chairs.
- Designs for the sub-structure of a parklet vary and depend on the slope of the street and overall design for the structure. The sub-structure must accommodate the crown of the road and provide a level surface for the parklet. Pedestals spaced under the surface and of different heights are a common application. Another method is to provide steel sub-structure and angled beams
- Parklets should use a slip-resistant surface to minimize hazards and should be accessible to wheelchair users.
- Buffers should be capable of withstanding at least 200 lbs. of horizontal force
- Parklets should include plantings that provide year-round interest. Native and drought-tolerant plants are preferred although other plants may be considered during the review process.
- Bicycle parking is strongly encouraged. Bicycle parking may be incorporated as a part of a Parklet or installed adjacent to a Parklet as a bike corral on the street.
- The use of local and sustainable construction materials is strongly encouraged.



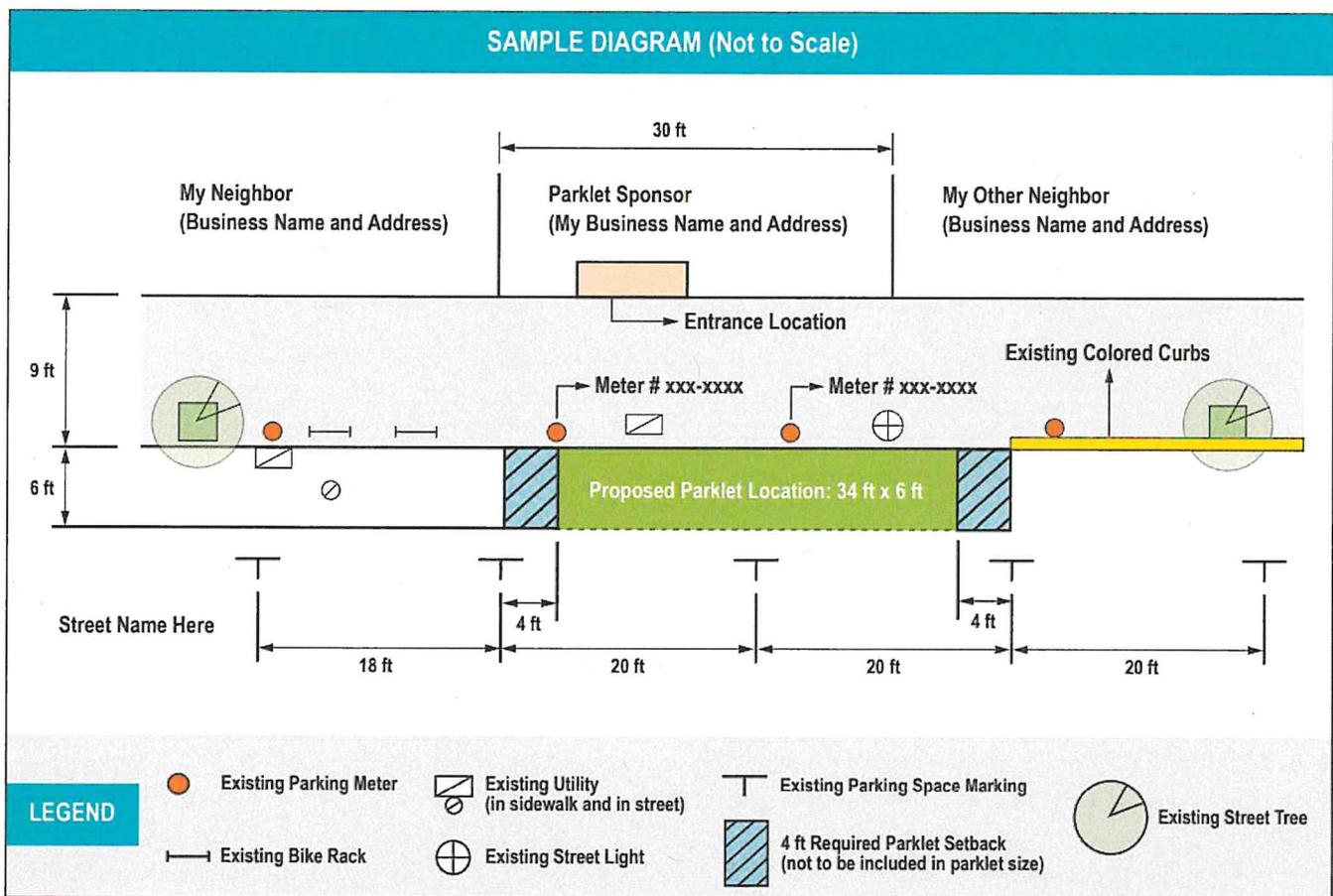
PARKLETS_{continued}

Obtaining a Parklet Permit

- **STEP 1: Confirmation of Proposed Parklet Location**
 - Anyone interested in establishing a Parklet should obtain preliminary confirmation of their proposed location from the City. This is required prior to submitting a formal Parklet application so the applicant understands where a Parklet can and cannot be located.
- **STEP 2: Demonstrate Neighborhood Support**
 - Demonstrating support for proposed Parklet projects is an essential part of the process and should be included with the initial Parklet application.
 - **Applications which do not include documentation of community outreach to the adjacent businesses, business association, and/or neighborhood association will not be considered.**
 - At a minimum, applicants must demonstrate the support of any businesses and/or property owners which front a proposed Parklet location.
 - Full consensus in support of a proposed Parklet from every property on the block is not a requirement for a proposal to move forward, but it's important to demonstrate adequate community outreach.
- **STEP 3: Submit your Parklet Application**
 - Prepare and submit a complete application package through the City's online application portal. (eproval.savannahga.gov)
 - Applicants must complete all required sections of the application, and pay the non-refundable application fee, prior to submitting the application for review by the City. The online permitting portal will walk applicants through the process step-by-step.
 - Incomplete submissions will be returned to the applicant for more information prior to review.
- **STEP 4: Application Review**
 - Follow the digital review process through the online portal. Applicants will be notified each time action is taken on their application.
 - Applicants may submit any required revisions throughout the review process.
 - The review process varies based on location and request, and may take up to six weeks to process fully.
- **STEP 5: Approval**
 - The final permit will be available to the applicant digitally upon approval of the application and payment of all required fees.
 - Once approved, the requested parking space(s) will be allocated to the applicant.

PARKLETS continued

Sample Parklet Diagram



The diagram above is an example of how your parklet diagram should look when submitted with your application. There are many programs that can assist you with creating a diagram. One of the easiest options is to make the diagram in MS WORD or MS POWER POINT so that you may use the shape features in the program to easily lay out your design.

PARKLETS continued

Parklet Fee Structure

Application and Permit Processing Fee	\$300.00
Parking Space Rental First Space / year	\$3,000.00
Parking Space Rental Second Space / year	\$1,500.00

The fees listed above will be assessed during the permit application process. A \$300 processing fee is due upon conditional approval of your parklet application. Yearly rental fees shall be billed as shown for up to two parking spaces for the parklet. ALL fees must be paid prior to any interruption to the parking spaces requested.